

Issue No. 860

October 2023

The News Sheet

North London Society of Model Engineers

October 2023



You can see this News Sheet in
colour by visiting our web site at www.nlsme.co.uk

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Front cover; - The garden railway has been running throughout the month and this picture of a splendid model of I.O.M railway engine No16 called Mannin. It was the most powerful 2-4-0T locomotive on the line and used to haul the heavy Port Erin boat train, a job which had previously taken two locomotives either double-headed or banked.
Photo by Billy



After reading this month's News Sheet you will appreciate that September was a very busy period for the club.

I hope you enjoy the read and I would like to thank all those who have contributed. Without your help this news sheet would not be possible.

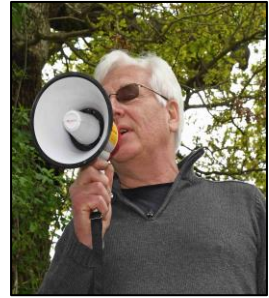
If you have any pictures of club events or have an article of interest, please send it in. All contributions gratefully received by your editor.

Chairman's Comments

Les

Well done Owen and David for a highly plausible explanation of the mystery photo on cover of last month's news sheet. (See *P41 ed*). Obviously a backhead but Owen and David note that it appears to be inside a van or carriage therefore, and most likely, it was part of a traveling training train. It looks a bit LMS to me. As to how, when or where NLSME visited remains a mystery.

Here is a picture of the assembled throng.



A reminder the author of a new book on Curly's life is seeking photos of LBSC designed locomotives or LBSC's own built locomotives. If you have any that you are prepared to offer, publication not guaranteed, please let me know.

NLSME Council is keen to move on with improvements to the raised track (RT) steaming bays. Hence my chairman's comments are short this month to allow space for reintroducing the topic below.

RT Steaming Bays and Carriage Storage.

By Les

Members review and comments are welcome.
Meetings will be held at HQ and Tyttenhanger to discuss.

We have long been planning to improve the RT steaming bays, many will recall a scheme to have them updated for the year 2000.

Well, some progress has been made, though along different lines to last century's proposal. A new proposal was agreed, in 2011 I believe, then reviewed at the February 2017 and February 2020 meetings at HQ. We have since gone part way to its implementation in so far as the containers, RT locomotives storage space plus new base and partial steelwork for new traverser have been installed. However, we have also provided a machine shop with lobby and a washroom. The lobby and washroom were not in the 2011 plan but are essential prerequisites to removal of the concrete workshop, freeing up space for the new steaming bays. Also, since 2011, many members advocate retention (albeit upgraded) of the existing steaming bays and traverser, being ideal for smaller locomotives or larger ones with drivers' trolleys.

Still with me? The upshot of all this is that members old and new can be forgiven for not knowing precisely what remains to be completed in the 2011 plan.

Council consider that clarity is necessary to ensure majority of members still support the objectives. Accordingly, the plan (back page) included with this news sheet gives you a full overview of the 2011 proposal, plus developments on site since then.

I must emphasize these are for your review and comment.

The next stage will be a large commitment of club resources and members time. We want to ensure the majority of members are supportive of whatever proposal is to be implemented.

Before viewing the plan, it is important to recall what we are trying to achieve, a list of requirements if you like. The question is, does the plan meet our needs or are amendments required? I do not recall seeing a full list so council members have drawn up what is likely to be the full requirements. The items are not in order or priority though the first four items were, I believe essential elements of the 2011 scheme.

The list is on the next page;

Requirements for the RT Steaming Bay Project.

(The list is not in any order of precedence or importance)

- Provide a roof over most if not all the bays
- Provide arrangement that facilitates coupling of loco/tender to one passenger vehicle forming a train within the steaming bay area
- Provide means of transporting an assembled train onto the running line with minimal time/disruption
- Provide storage for six passenger vehicles (four to remain at station for making up longer trains)
- Train transportation system to be simple and manually operated
- Provide signal and safety interlocks for transporting system
- Retain existing steaming bays and remove rails that are no longer required (mostly 2.5" gauge)
- Modify existing steaming bays to allow "drop in 3.5" rails"
- Retain and renovate existing traverser
- Increase number of 5" gauge steaming bays, to enable grate insertion and dropping
- Provide storage for;
 - Coal – Oils – Air compressor – Power transformer – Vehicle couplings
- Retain or replace existing trolley for loading/unloading from cars
- Provide loading/unloading ramp for cars
- Retain locomotive access into container No 7
- Each bay to have 12 and 24v power supply
- Each bay to have water supply
- Each bay to have air supply
- Provide overhead lighting for the bays
- Ensure all concrete flooring is fit for use, relay as necessary
- Allow ease of access to machine shop and wash room
- Upgrade CCTV system
- Provide storage areas, off the ground, for tool boxes etc.
- Provide seating area
- Provide secure location for site key cabinet
- Provide mobile waste loco ash collection bins mounted a metal frame with wheels / castors.
- Increase space between steaming up bays
- Raised benches for tools etc. adjacent to each steam up bays
- Update or revamp outer steaming bay fencing.

We will give interested members the opportunity to discuss all the above at the November general meeting at HQ and at Tyttenhanger during November.

Treasurer's Report

By Mike



The last Council meeting was pleased to accept an application to rejoin the Society from the Michael & family. We are very pleased to see them return to the fold after a break of a number of years. Along with Michael, the two boys, Thomas and Robert are now grown men and are keenly involved in miniature railway operation. It has been very good that they have during the summer been able to help out our Fetes and Fairs Section at some of their events.

At the same meeting we were also pleased to welcome Terry back into the Society. As mentioned in last month's News Sheet, many members will be aware that his father-in-law was Geoff Cashmore, a long-time member of the Society until his sudden passing. Terry has very generously passed over to the Society all of Geoff's collection of films, photos and negatives, much of it documenting NLSME history and events, together with hundreds of railway photos of exceptional quality. I remember in my youth with the Society, Geoff selling, in his opinion, sub-standard quality 1/4 and 1/2 plate pictures at meetings for 6d each. I have a few myself, including a pair of pictures of A3 'Donovan' 60047 starting away from KX, hence why my own 'OO' A3 bears that number.

In October last year's issue, I mentioned that the gates into the HQ site were being motorised with a suitable keypad to operate them. As at time of writing this article they have still to be brought into service., so our neighbours on the site have continued to maintain a 24/7 gate security guard, which from our point of view does ensure that our security is good.

At Colney Heath, we have been fortunate that this Summer the rabbits and occasional droughts have kept the majority of the grass areas trim, whilst waiting for the chosen replacement rotary mower to arrive in the country.

This winter we must now progress the replacement of the garage doors of the garden tool shed, broken into in January, so that we have a secure location for the various new pieces of gardening kit being sourced. Winter working parties begin in November and are keen to see volunteers new and old getting to grips with the myriad jobs, small and large which have to be attended too. This will ensure our site is in tip-top condition for next year's running season.

Meanwhile enjoy the rest of the running season and get ready for the 6th October General Meeting, which will be about your Work in Progress. Bring something along to show and discuss.

Keep safe and keep engineering.

Forthcoming General Meetings 2023.

By Ian

All meetings are Friday evenings at HQ starting at 8pm.

The Programme at the moment reads thus: -

Friday 6th October 2023. Work in Progress.

Members, please share with the Club your recent Challenges in the Model Engineering World. Rail (all gauges), Marine, Slots, or Fetes and Fairs are all very welcome.

Friday 3rd November 2023. Presentation of the 2011 plan for steaming bay improvements and open discussion on these. See also Chairmans comments in this issue. Open discussion on any other club issues members want to raise, time permitting.

Friday 3rd December 2023. A pre-festive gathering at the Summer's Lane HQ. the Clubs way of saying 'Thank you' for the sterling work done by all members during the Year.

Friday 5th January 2024. Yes, a new year has started! In the 'Old Days' we used to talk about what we had seen at the Model Engineering Exhibitions. But those days seem to have passed so perhaps we can show illustrations of THIS past year's glories.

Friday 2nd February 2024. Welsh Highland Railway Centenary. Owen Chapman will talk about this fascinating Railway. And, of course illustrating it with his magnificent photos.

Friday 1st March 2024. Building a workshop from scratch. Peter Seymour-Howell will, we hope be able to talk about planning and building a work shop for model engineering.

Friday 5th April. 2024. General meeting. Topic yet to be confirmed

Please note; -

I would very much like to hand the role of organising the General Meetings on to another volunteer. I have had the great pleasure of organising these Meetings for many years. In fact, since the demise of the Locomotive Section Meetings.

I would like to think that the future General Meetings are in good hands and I wonder if there is someone willing to take over and so carry on a tradition that it seems few other clubs can sustain.

Any questions regarding the meeting contact, Ian



1st September – General Meeting

By Dudley

Members were given a presentation from Professor Tim featuring a 'Close – Up' look at The Model Railway Club's 2mm Fine Scale 1930's era Copenhagen Fields model railway layout.

Tim is President of The MRC which is based at 4, Calshot Street, London, N1

The layout was planned and started back in 1983 and the project is based on the approaches to Kings Cross Station set in the era between the two world wars.

Most of the locomotives which run on the layout are scratch built to 2mm Fine Scale standards and Tim displayed two finished steam locomotives plus one in process of building.

Tim also demonstrated various construction techniques for buildings based on the layout with actual period pictures of the real subjects photographed around the area modelled.

The layout is named after the park that sits on the top of Copenhagen Tunnel, the second tunnel, about a mile North of Kings Cross Station. The name Copenhagen Fields is from the Ambassador of Denmark's residence in the 18th. Century. Nowadays this area is where the entrance for the Channel Tunnel Rail Link that emerges from its tunnel to terminate at St. Pancras International Station.

The evening was very interesting and gave various railway modellers present to wonder how this very high level of detail can be personally achieved.

The pictures that follow really don't do justice to the amazing level of fine detail within the models. These pictures (by Owen) will be best appreciated in colour. They can be viewed on the NLSME web site.





York Road tube station entrance





Raven NER 4-6-2 class (A2).
2mm Fine scale,
9.42mm gauge.

Derelict corner



6th September – Swiss Gauge 1 visit

By Geoff

Highlight of September was the visit from the Gauge 1 Swiss Group.



Swiss members of G1MRA have undertaken a visit to a number of Gauge 1 layouts in the UK at approximately two-yearly intervals since year 2000. These visits are a co-ordinated coach tour around the UK with visits on successive days to Members or Club layouts plus a few visits to preserved railways.



Their visits have included the NLMSE track a number of times and we look forward to each visit. This year's 17-person coach tour included 6 tracks, in Norfolk, North London and Kent plus the North Norfolk and Bluebell Railways.

They each bring a model locomotive and as expected a number of these are of European prototypes. This year we enjoyed SBB A 3/5, Bayerische S 3/6 and Gotthardbahn A 3/5, plus a number of UK prototypes.

You cannot fault their enthusiasm as most ran their locos two or three times during the day.



They were supported by a significant number of our members throughout the day. The tea and conversation flowed and we were sustained by a wide range of snacks.

We enjoyed the day and look forward to their next visit and a time to meet old and new friends again.

We were even invited to visit them, perhaps we should hire a coach and arrange our own tour of Switzerland.

9th September – 3½" Gauge running day



Mike & Jean celebrate

The 9th September was a day for celebrating the Diamond (60th) wedding anniversary of Mike & Jean. They were joined at Colney Heath by family, friends and locomotives.

A rather splendid cake was also shared with everyone. If you look closely, you may be able to see even the cake had a railway theme with the silhouette of a golden Lion on top.

Trevor (Secretary of Fareham & District Society of Model Engineers) presented Jean with a bouquet to celebrate her recent 80th birthday.





Visitor Steve brought his 'Lion' specially because he was aware of its significance to Mike.

Mike first watched the film *The Titfield Thunderbolt* more than 70 years ago which kick started his involvement in model engineering.

Photo by Mike & Jean's daughter Barbara.



Trevor's granddaughter Bethany driving his loco.

There were 15 visiting locomotives on the day to add to the celebrations. Limited space only allows a few pictures of the day to be published.

A good time was had by all and messages of thanks have been received for the welcome extended by members NLSME.

10th September – Boat Regatta

Unfortunately, a date clash resulted in the vintage yacht members not being able to attend the regatta. But the day still proved to be popular with NLSME members from the marine section and a number of visitors. As a result, plenty of boats and yachts were in evidence as can be seen in the pictures below from Steve & Owen.



10th September – St Marks Church visit

On Sunday the 10th September we opened the gates for what has become an annual visit from our nearby Church neighbours.

A considerable number of families arrived bringing their picnic's and enjoying rides on both the raised and ground level railways.

It was a very enjoyable day for all who attended.

They have already asked if they can come back next year.

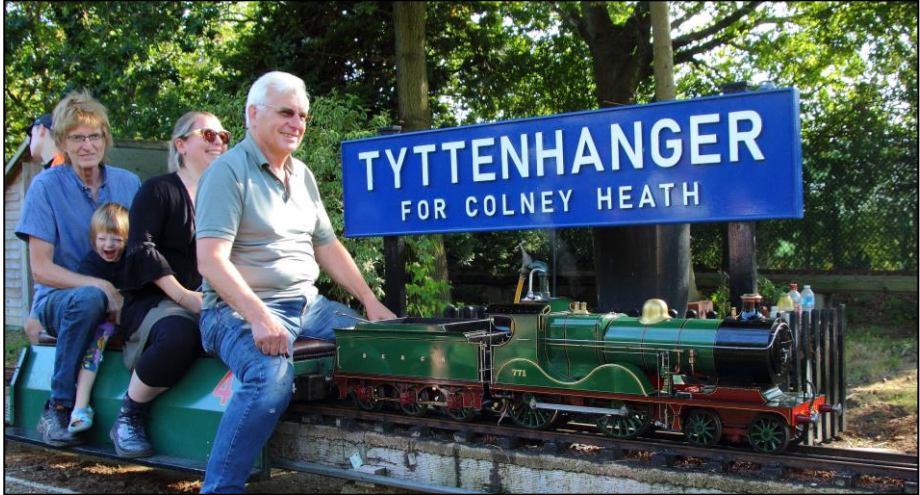
Pictures by
Owen



September Public running days

The two public running days were very different the first was a sunny afternoon with plenty of visitors the second can at best be described as a dull weather day with a few hardy souls.

3rd September – A bright sunny day with a good number of visitors made for a great afternoon. Our chairman steamed up his recently completed L class loco and used it for the first time for public running.



17th September – All that can be said about the second event was that despite the weather visitors still arrived and some very wet and committed drivers kept the trains running all afternoon. The GLR was suffering from poor adhesion and ended up double heading with a banking engine on the rear. Our thanks to the drivers who kept things running getting rather wet in the process. I think the pictures below really sum up the day.



23rd September – Keech Hospice family day

This was a rerun of the event held in July when very few attended due to the heavy rain which spoiled the day. Much better weather this time resulted in a good number of families with children from the hospice enjoying many rides during the afternoon. Both the ground level and raised track were in operation.

Jenny from the hospice who organised the event has asked me to pass on the sincere thanks of all the families who came. One boy who came with Jenny had not been outside his house for some considerable time. He has Asperger

syndrome and had lost most of his hair as a result of chemotherapy. He didn't want to come and was very quiet on the journey to Colney Heath. But he left having had a great afternoon. Jenny said he was a different person on the journey home.

We provided not only an enjoyable day out for these children but also for their siblings and parents.

Thank you to all who helped. I know you had an enjoyable day too.



Brian is back!

For many years Brian has been one of the clubs most regular drivers. But he has been absent for quite a while. It's great to see him back again in good health and driving his locos. We all wish him many more years steaming and offering his practical advice and encouragement to those less experienced amongst us.



For Sale

Brazing hearth

£40.00.

Please contact Geoff
Hammond

07968 854472

or

geoffrey.hammond@ntlworld.com



Building My Class 20 Electric Locomotive

By Nigel - Part 2

Right dear reader now where did I leave you at the end of Part 1. Ah, yes, the visit to Dan Jeavons house in Kidderminster to pick up the castings which I had ordered.

I have tried to work out when this was and the best year, I can come up with is 2004 – based on a label from a company called Parvalux (still in business) on a cardboard box Dan gave me with some of the castings in. Dan told me he was buying motors from them for the new range of models he was working on.



Finding his bungalow was not easy. I think I might have had a sat-nav but I remember that nowhere could I find his address. Luckily, I asked someone and I was pointed to go down a cart track, turn sharp left and along there was his bungalow and a few other properties.

Dan and his wife made me most welcome with a cup of tea and then he said right let's go and get your castings etc. As we made our way down the garden there was a low brick building at the far end. Dan explained that it was an original hovel where a family would have been expected to live. Entering through the low door it was very dark inside until he switched on the light and there in front of me was a GWR Large Prairie the only difference was this was electric powered! This was the flyer he gave me. I also seem to remember that he said he was also working on an electric powered City of Truro.

DETAIL in DESIGN

Available Soon!
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• Class 20 (Chopper)	• Class 47	• Metropolitan
• Class 35 (Hemel)	• Class 52 (Western)	• 10 Ton Wagon
• Class 37	• Class 55 (British)	• Duck Shunter
• Class 40 (Whistler)	• De-Wispex (Puffin)	• P16 1 Coach

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Anyway, returning home with a very long aluminium main casting, a very near equally long fibre glass body and a cardboard box of other castings and laser cut parts for the loco made me realise that I had a fair bit of work, and probably years, before I would see a finished loco based on my usual slow rate of progress due to work, family and life in general. But at least I had bought the main castings now where to store them?

My workshop is slightly less than half of a double garage – space had to be left at the front for the family bikes and the garage door to be able to open (no longer possible due to storage of other items). I'm sure dear reader that you may well have experienced the same in the pursuit of our wonderful hobby? The main casting ended up propped up against the tall cabinet of drawers where I keep tooling etc. The fibre glass body was stood on end and the cardboard box ended up in front of the lathe where it still resides to this day!

So what progress was made then you may well ask? Nothing much according to the photo's I have until November 2013 by which time I had taken early retirement and I obviously decided to do something with this new found freedom. The main casting was put on my Warco Mill/Drill and the lugs for the



fibre glass body were machined to ensure a snug fit and a light cut taken to clean up the surface the body would sit on. Ah progress at last I hear you say, well yes but then I became the proud owner of my 5" Gauge Horwich Crab and

I'm guilty of relegating the main casting back to its former home propped up against the cabinet.

Fast forward to 2020 and Covid struck so any normal activities ceased. After repairing the pergola in our back garden my wife Helen suggested that I make a start on assembling your electric loco. I don't think she realised at the time that assembling anything was way off and manufacturing would have to come first. So, I decided to start with something easy and spied in my cardboard box of bits in front of the lathe the equalising bars for the bogies. These were supposed to be brass lost wax castings which had been bought from another source but when finally received after a very long wait looked anything but. As soon as I started machining one of them it ended up bent like a banana which then took an age to resolve but eventually, I had four complete equalising bars



with attached lost wax equalising bar spring retainers and trial assembled Success, so what next?

I think we shall leave it until next month's exciting!!?? instalment.

Part Built 3 ½ Gauge Virginia

I am a very old model engineer so must finally give loco building. Does anyone want a half-built Virginia loco all parts are there, all copper most machining done free to genuine builders' email (dodger1504@gmail.com) for more information.
Roger

Bookworm Writes

Following a post-lunch nap, the other day; I came too finding the sitting room in darkness. Strange I thought I can't have been asleep that long. Looking into the gloom a little harder I realised the premature darkness had been brought about by the closing of the sitting room curtains.

Just at that moment the familiar rotary click-clunk sound of my owners Hanimex slide projector drifted up offering the reason for their closed state – Now for my younger readers, Hanimex was an *old school* 'must have' make of slide projector and was what people born after 1990 would call a 'low tech' device for showing your favourite photographic slides and transparencies projected onto a screen or wall (even an old bed sheet), providing hours of pleasure and entertainment by producing a picture anything up 8 feet square; from just a light bulb, a magnifying lens and a bit of electricity. In fact, so big could the image be you literally felt 'in the picture'. In today's hi-tech world you now need a fancy digital source, an expensive computer AND another box of expensive electronics containing: a light bulb, a lens and using even more electricity to produce the same thing ...but that's called progress....

Anyway, within the darkened room a waft of tobacco smoke swirled up and past me reminding me that once again we are moving closer towards autumn and the longer darker evenings. The winter ritual of sitting with a mug of tea a pipe of tobacco and a box of slides looking at holidays past and thinking of 'what might have been' will be one of the ways my owner will fill his time over the coming months.

The problem is my owner has a weak heart. He just cannot resist temptation no matter what he has on the go in the workshop. For practically every time he goes away on holiday, he loses his heartto yet another project.

For example, there was the time when he visited the Dart Valley railway back in 1967 when all that was visible then at Buckfastleigh Station was a Collet designed engine no 1466 under a thick tarpaulin, a couple of old GWR coaches (including one that had a decidedly dodgy door lock that had him trapped inside for a time....laugh!), a Toad brake van with the red and green flags still in situ and an old 'Wickhams' rail trolley; other than that there wasn't really much happening at all. But then IT happened, LOVE. In a siding he came across pannier tank no 3369 the rare one with outside cylinders, and he thought it the most wonderful thing he had ever seen. Over the scampi and chips in the pub that evening he told Mrs Owner that his next engine would be an LBSC 'Pansy' design modified to have outside cylinders. Upon their return a boiler was ordered plans were purchased and a castings list obtained.

Well, that was over fifty years ago now and as still no metal has been cut, I think we can safely put that one down to a holiday infatuation – boiler anyone?

Then there was the trip to the Isle of Man. Seven glorious days spent touring the island Douglas to Port Erin retracing the islands railways. Like many people before him visiting the island, I don't think he had given much thought to the railways there, believing them to be part of the British Railways system. Finding them to be nothing to do with British Railways and being not standard gauge nor *really* narrow gauge, he was intrigued to find that most of the island's engines had been built by Beyer Peacock and bore a remarkable resemblance to (albeit 3ft instead of standard gauge) to the ones supplied to London's Metropolitan underground railway.



Several good sessions in the loco shed at Port Erin followed by photos taken from every possible angle, he finally felt he had enough 'research' material. Mrs Owner however was less than enthralled and was barely two shutter clicks away from heading back home without him after missing out on seeing the 11th century old stones of Peel Castle, just so he could get that *really important* picture. Possibly (and tactlessly in my opinion) describing one of the engines as looking rather seductive against the backdrop of the green landscape did him no favours either.

Back home and he found that Mike Casey had come up with a design for one of the islands engines in 5-inch gauge, so a boiler was ordered plans purchased

and a castings list was obtained. That enthusiasm sustained him for more than one winterthen his thoughts turned to the next holiday outing ...Another boiler anyone?

North Wales on this occasion to see the Ffestiniog railway which was new to him; despite all his years being a volunteer on the Talylyn railway. There he saw the narrow-gauge Alco Baldwin 2-6-2 loco 'Mountaineer' for the first timeand yes, you've guessed it; it was love at first sight. He nearly cleared the little gift shop at Ffestiniog out of film for his camera he took so many photographs of it.

Once back home, all the 1973 ME back numbers were dug out to find Don Young's series on building 'Mountaineer' in 3.5-inch gauge. A boiler was ordered plans were purchased and a castings price list obtained. This I think was probably the closest he ever got to really committing himself to an extra-material project; he had even decided whether or not to fit the 'spark arrester' chimney. Then just as he was about to place an order for the frame steel, wheels and cylinder castings he looked again at the drawings, this time more carefully. 'Half inch frame plate material will be required' (to form bar frames) "*half inch!*"Well, he must have mullered and muttered, and mullered for well over an hour before finally re-reading Don's spec for the whole loco in Model Engineer: 36 inches long, cylinder bore 1.5-inch weight approximately 200lbs ..."*200lbs, he said for a 3.5 Inch gauge loco!*" Well, that settled its immediate future... as well as its long term one. That too was consigned to the '*maybe I'll build it one day*' list – Yet another boiler anyone?

The last batch of slides that afternoon came from an altogether calmer time... last year. **Bookworm Note:** yes, you can still buy slide film for those like my owner not wishing to enter a photo-shop, dangle a stick or live under a cloud – When he and Mrs Owner holidayed with his cousin and family in the Lakes near Windermere. *Good* I thought there will have been no temptation for him on that trip. Just then the last slide came up and was of a steam launch... Did I imagine it or did I hear him say, "*Aren't those lines wonderful, I've always fancied owning a steam launch, I wonder if you can still buy boilers for them?*"

The moral of this tale for those with similarly weak hearts has to be that it's always better to stick with your current love and not to go losing your heart every time temptation comes along, no matter how alluring it may be....it will be cheaper in the long run.

Anyone in need of a boiler.... I know where there are a couple or three....or.....?

For Sale

I have a Frem FFZ-400N scroll saw for sale. Good working order except one side of the guard is broken and will not tighten. might be repairable. £50.

For more details email Jonathan



Coal, Steam Oils and Lubricating Oil for Sale

Coal: Anthracite Beans in 25kg bags

The size most used by our locos. £14.50 a bag.

Anthracite Grains in 20kg bags

Suitable for 3 ½" gauge and small fire-hole doors. £13.00 a bag.

This coal is for private use, the Society supplies coal for locos that are used for passenger hauling on public Sundays.

Contact the Treasurer – Mike

The first Lesson

During September Paul kindly offered to give Cheryl a few lessons on how to drive a steam locomotive. Things went very well and with a little more practice I think we have a new driver to enjoy the railways at Colney Heath.



(Picture by George)

5-inch gauge A4 Silver Fox for sale

This locomotive was a commission build and owned by deceased member Reg and subsequently inherited by his nephew who has no engineering skills.

The locomotive is complete (including side valances) but in a partially dismantled state. It has a commercially made boiler. The loco requires the attention of an experienced model engineer to rebuild the engine and correct the known faults. Silver Fox has been valued by a reputable dealer and offers in excess of this valuation will be considered. For more information contact Keith



The Geoff Cashmore Photo collection

A large collection of photographs have been donated to the society by the family of Geoff Cashmore. We will publish some of these over the coming months. The full collection once sorted and catalogued will be available at HQ.

This month's pictures all feature Bert Mead.

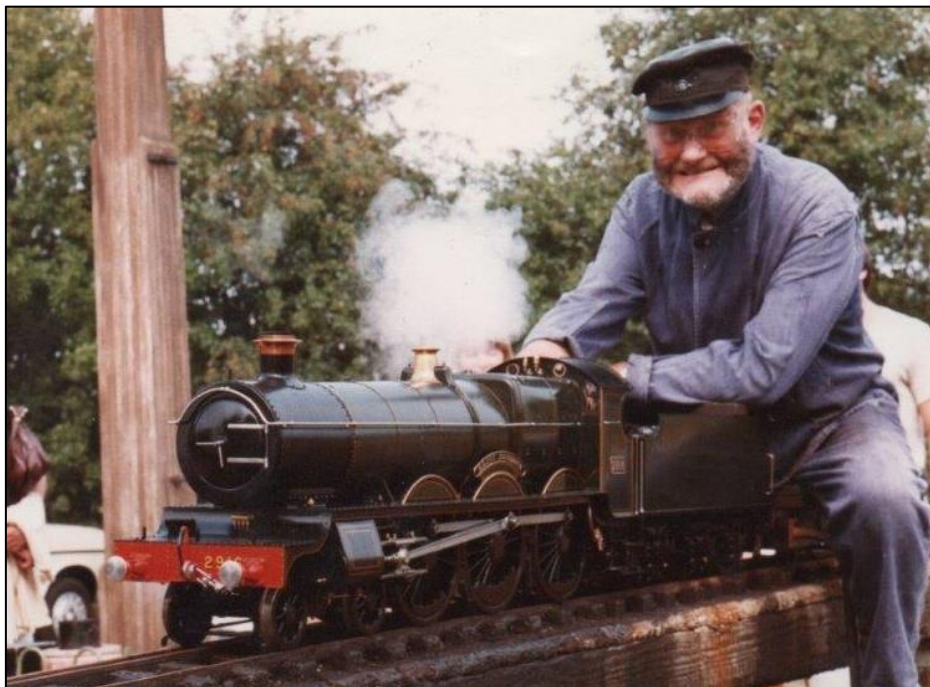
Here he is driving Geoff's Juliet on his up and down track in his garden in Hampstead.



Bert driving Paddlebox on LBSC's own railway at Purley Oaks



Bert driving the Paddlebox Back at Arkley on the original back straight after surmounting the climb back from the extension.



And in this final image, Bert as many will remember him driving St Benedict.

Narrow Gauge News – September

By John

A warm autumnal greeting everyone and welcome to this month's narrow-gauge musings. By the time this newsletter drops onto your doormat the public open day season will be almost over, that's another year flown by!

In comparison to previous years, I don't think we have had the numbers of members of the public like we have had in the past, no bad thing as it seems to create a more pleasant atmosphere for the public and club members alike.

As ever on the narrow-gauge garden railway we have enjoyed running to entertain our visitors and of course to please ourselves!

We have had some very enjoyable sessions recently and in time honoured fashion I am going to let the photos do the talking, and let's be honest we would all rather look at pictures of trains than my ramblings wouldn't we.... Enjoy!







My model boat collection – Part 12

By Roy

Celia May, this is a copy of Bowmans Sea Jay.



This is another model boat that dates back to gazing at the unaffordable in Beatties shop window. It was a Bowmans Sea Jay, 32 inches long basic steam river launch. It was steam powered as mine was to be, but often an electric version was available too. Forty years later Model Boats magazine (February 1988) came out with a free plan called Celia May (plan MM1429), and it is still available. Balsa was the building method suggested but being me, I went for whatever was available! Inevitably it was a Sunday, no shops open! A search of the garage was needed.

The sides are 3/8ths inch thick and I found some 9-ply resin bonded ply, however, this is very stiff, only solution was to steam it! Now I have never steamed wood before. Another search found an old electric kettle that did not turn off when it had boiled. I had some plastic gutter down pipe about 3 feet long, which I blocked at one end. I made a crude jig and drew the correct curve on a strong piece of wood and bashed in the nails. I thought I might have to lever to shape and had no idea if the ply would bend or split apart!

I started the first steam-up and after 5 minutes I brought the very hot piece of ply out and it was floppy at the end!!! I bunged it into the jig made sure it was vertical and within 10 seconds it had set to its new shape. Next piece went in to the steamer and I reckoned the wood would have to go in the jig the other way up. Don't want two left sides do we!

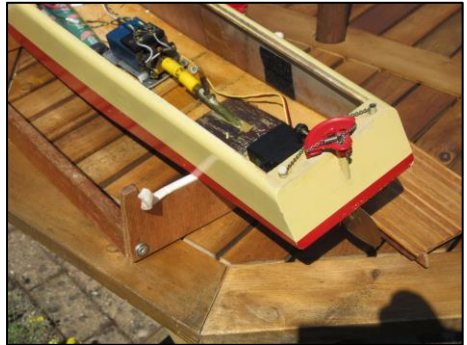
After the allotted time a quick grab with some Stilsons and into the jig. It all worked and went smoothly. I could now cut the sides to finish shape and trim to length. I also had to cut an inside rim around the top of the hull sides. Ammonia will work, but is bit dodgy to use, and for very thin wood, window cleaning spray works well, as there is ammonia in that too.

The hull bottom was mainly flat but rises up after 2/3rds. of the length and then I fitted the transom. The inner groove cut into the top of the hull supports the 3 pieces of aluminium super structure. It was a departure for me but I had some aluminium sheet and cut all to shape.

Bending aluminium has its own problems and after a partial split along a bend I went to see my metal work Guru, Malcolm Beak. He imparted methods used which I vaguely remember as involving soap and lots of heat and a hammer! When I got back some of my aluminium splits were never going to hold, so brackets were cut and shaped and glued using araldite and they have held all this time.

The little door entrance arrangement at the stern is an exercise in bending tinplate, additional items are the brass port holes and cut down plastic cowl vents and the searchlight, which does work. The metalwork was painted with an etching primer. A recent trip to Poundland in Watford revealed a metal etch spray, Guess how much? Yes, it was £2.50, that's inflation for you! Most of my glues and varnish comes from there and it is all very good.

The plans for the rudder did not involve radio and was just a pressure-controlled tiller which determined direction and this had been bothering me. Then I came across the chain operating system for the rudder in an old tugboat. You can see from the photo how it works. I sawed a Meccano 2-inch pulley into about 60-degree sections. The top part retains the boss and the other section soldered beneath. There is a small spring on top to give some tension to the chain. The chains vanish into plastic fittings either side, drilled and filed to allow easy movement. The ends are connected to a servo arm.



Back then the deck was not what you see now as the deck was all aluminium and lifted off in sections to access the steam plant. I had a steam plant, a one-off item made by a friend which I paid for. At this time, I was distracted onto another model and some electronics I had designed in 1987-8. (This was the steam boiler water level control circuits that were published in a now defunct RC magazine. They are available on the St. Albans MES web site)

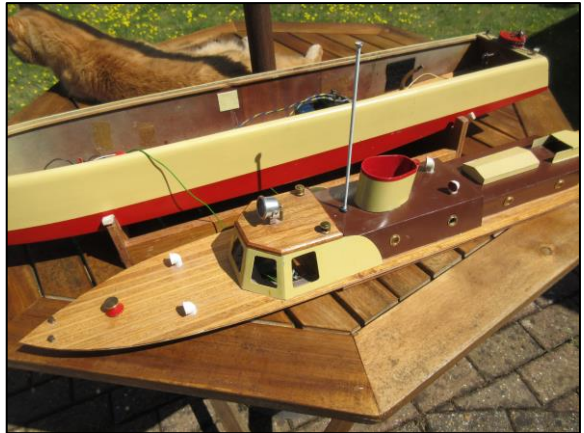
About this time a fellow club member and friend, Phil, had a stroke and was in hospital, his memory had almost gone but at the time I did not know this. I was unable to visit but wrote to him with an electronic circuit I had devised for a model he was making. I went through the circuit in writing to aid his understanding. When he got the letter, it was gobble de gook to him but he realised that I must have expected him to understand it.

With this in mind, he told me later, he persevered and worked it all out and he said it was like therapy to him as it triggered other things. When he got out, he was marooned at home and I went to see him and I took with me the steam plant and the virtually finished Celia May. I said I have bought some more therapy with me! Would he fit the steam engine into the boat please? He was bored and very pleased to do so and I left him to it.

About a month later he said you better come and collect your boat. I arrived, it was not really good news for me, as he said the steam plant is all too heavy to work in Celia. Further to this he said he had bought a hull which fitted in with a French class of minivap with a hull of 20 inches (50cm). Well, I had a quick think and decided that if the steam plant was not going to work in my Celia it would be put to one side and maybe not used. So, I said you better keep it in your boat and if you don't use it let me have it back.

So, Celia, sans power, came home with me. She languished for several months until I remembered an old Bassett-Lowke Marine electric motor I had bought in my late teens. But I did not have a satisfactory deck to suit electric power as it was not reasonably watertight. I finally decided that I need a whole deck that comes off and this is what you see now.

I sawed off the aluminium deck sides on the cabins and located the cabins onto the new planked deck. I was very pleased with my first try at rolling the funnel to shape using a thick bed of blanket and stout dowel. The sort of whistle at the rear holds it all together with a twisted key piece to retain it in the cabin top. The aerial works and is insulated from the surrounding metal.



Off to my metal guru and he made me a brass winch with a threaded rod inserted while I waited. I made the fitting in the boat to receive the screw which holds all in place. It is the red painted item at the bow, it might be dark grey in the Newsletter!

I made my own propeller, a shape formed with a compass and soldered onto a thick 4BA nut and a shaped locking nut to the rear, I was surprised how little balancing was needed. Now for the final set up and off we went for the maiden

voyage, probably about 2 years after the steaming part. I was a bit disappointed that she was not that fast, I do like a little bow wave etc.

So back home and a sort through the props I have and the one fitted now is just right. I have swapped a few motors in and out over the years as I can use her as a test bed. She has a second Marine motor at the moment, this one discovered as a central part of a very old rudder mechanism from a donated boat which had been dumped in the spares box the motor unnoticed for several years.

The model is substantially very similar to the original, it is the colour which was the problem. The nearest one to the original was called 'County Cream', no longer available and after fruitless searching I settled on the pale yellow and red beneath. The paint job is as it was in 1989 and just kept clean. A thing I remember from school is a good finish needs good preparation. A basic rub down wet and dry with varnish several times to seal the wood surface, an undercoat and several final coats all rubbed down wet and dry. I also use a quality furniture polish at the end which is what you see now.

TRACK STEWARDS ROTA 2023

1 October 2023

Derek – Senior Steward

1. Derek
2. Tim
3. Craig
4. Michael
5. Peter
6. Rob
7. Timothy
8. Peter
9. Christopher
- 10.

Ground Level Despatcher
Steve

15 October 2023

Keith – Senior Steward

1. James
2. David
3. Susie
4. Billy
5. Graham
6. Joe
7. Max
8. John
10. George S.

Ground Level Despatcher
Steve

Another model from Ron's Workshop

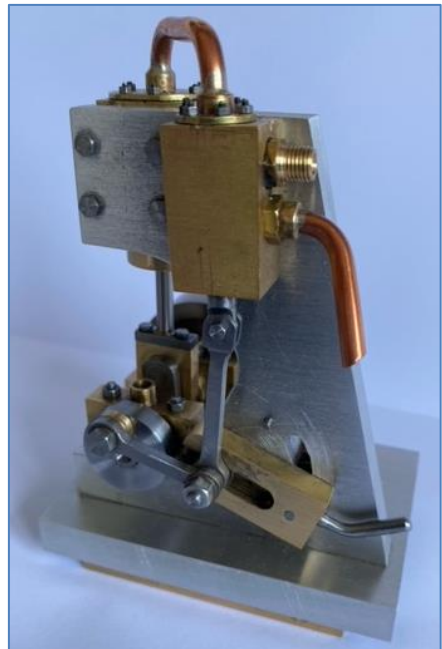
One day whilst sitting drinking tea in the coach at Colney Heath Ron was flicking through a few old copies of the Model Engineer. One article in the January 2006 edition grabbed his attention. So, he took it home to study further. It was an article by Colin Pape entitled Designing a Hackworth Valve Engine.

Hackworth had many achievements perhaps the most famous of which is his locomotive design Sans Pareil the locomotive he entered in the Rainhill Trials. Unfortunately, although it ran well it broke down and the winner of the trials, we all know was Rocket.

Like the article's author the story of Hackworth it inspired Ron to build a simple single cylinder engine with working reversing gear based on the Hackworth system. Parts 2 & 3 by Colin Pape gave details of this little engine's components.

Unlike most of us who would have spent many months deliberating Ron just went out to the workshop and in short time produced the fine model shown in the pictures below. If you would like to see the three articles or better still see the finished model just ask Ron who will be pleased show it to you.

It's been sitting in your editor's office for a few weeks. It's a joy to look at and as you would expect. Unfortunately, I now must give it back.



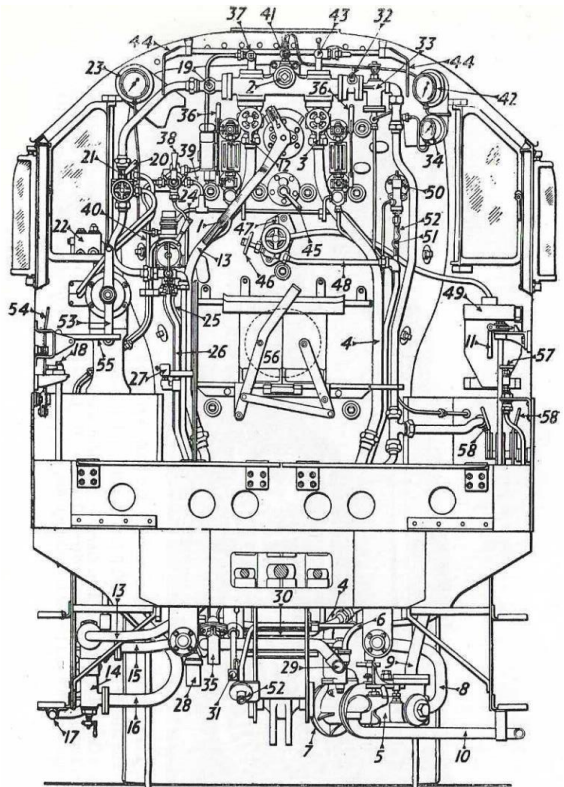
September News Sheet Cover Photo

By David (with help from Owen)

I don't know if the editor has been inundated with comments on the September cover photo from the G Cashmore collection - Les raised it on the WhatsApp group and Owen responded advising it was definitely LMS and noting that they built a number of mock-ups for training purposes etc. Since nobody else commented further I thought I would try and figure things out with reference to documents in my possession.

Firstly, this image from Locomotive Management 9th Edition by Hodgson & Lake (Revised by Lee) 1948 showing the cab fittings of an LMS class 5x locomotive. Very strong correlation with the cover photo although inevitably there will be detail differences. (This book also contains illustrations of backhead's of locos of other companies - and they are totally different.) Clearly full marks to Owen on that one.

Secondly, inspection of the NS cover photo suggests that it was taken inside a railway carriage - note the ceiling profile. Also, the mock-up backhead is cut off roughly where the window openings start on the actual loco. By locating the mock-up adjacent to the right-hand side of the carriage it was possible to position the doorway to the left of the photo and remain within the envelope of the carriage. Certainly, the LNER had at least one carriage fitted out with locomotive fittings for training purposes, (although I am not aware if it included a full backhead as here) so it is conceivable that this formed part of an LMS mobile training unit which could travel the entire system and be used for Mutual Improvement classes etc.



Thirdly the pipework appears to connect with the apparatus on the right-hand side of the photo - which includes what looks like a Driver's Control Unit for the British Railways AWS system. The diagram below is from BR Steam Locomotive Handbook.

If I am correct that would put the photo after mid-1950s when the BR system was widely implemented following the Harrow and Wealdstone accident. (Obviously the AWS unit location on the mock-up does not reflect that in reality - but for training purposes would have been adequate.)

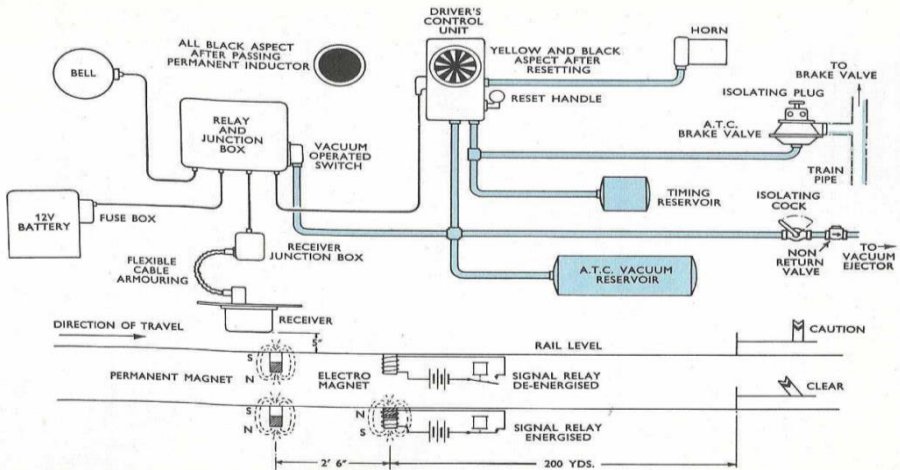


Fig. 89 AUTOMATIC TRAIN CONTROL
British Railways System

I hope the above is of some interest - Naturally if someone has already come up with a definitive response then feel free to ignore this, or just include any bits which might amplify what has already been stated.

Lastly about 30 years ago we had a talk by an ex LMS driver from Watford Junction (Alan Ashbury I believe was his name) - I bet he would have been able to tell us all about it. Just shows how information can be lost if not recorded.

Club Dates for your 2023 Diary

Every Wednesday evening HQ is open for the HO, OO, O gauge layouts and the Slot car section also meet. All welcome to join or just visit.	
Most Wednesdays the G1 group meet at Colney Heath	
Every Thursday; A mix of RT & GLR Loco running when track is available, working groups, and general conversation	
Every Saturday Ground Level Rly at Colney Heath	
Every Sunday; A mix of RT Loco running when track is available, working groups, and general conversation	
Every Wednesday evening HQ is open for the HO, OO, O gauge layouts and the Slot car section also meet. All welcome to join or just visit.	
October	
Sun 1 st Oct	Public Running 2pm to 5pm
Fri 6 th Oct	General Meeting at HQ from 8pm Work in Progress (See page 7 for details)
Tue 10 rd Oct	Council Meeting at HQ 13.00
Sun 15 th Oct	Last Public Running 2pm to 5pm
Sun 22 nd Oct	Working party at Colney Heath 9.00 to 12.30
Sun 22nd Oct	Deadline for copy to editor for November edition
Sun 29 th Oct	Working party at Colney Heath 9.00 to 12.30
November	
Every Sunday	Working party at Colney Heath 9.00 to 12.30
Fri 3 rd Nov	General Meeting at HQ from 8pm 2011 steaming bay plan & discussion (See page 7 for details)
Tue 7 th Nov	Council Meeting at HQ 13.00
Advance notice of other events in 2023	
If you know of an event not listed above, contact the Secretary to ensure it is added to the club calendar and also tell the news sheet editor	

A Non-council member, representing a section or committee, can, on request to the Secretary, attend council meetings as an observer or to submit proposals as set out in the club's constitution. If attendance is agreed the secretary will advise the member concerned.

Please notify our secretary of all meetings and other Society events for inclusion in the Society Calendar. Approval for special events still rests with Council.

